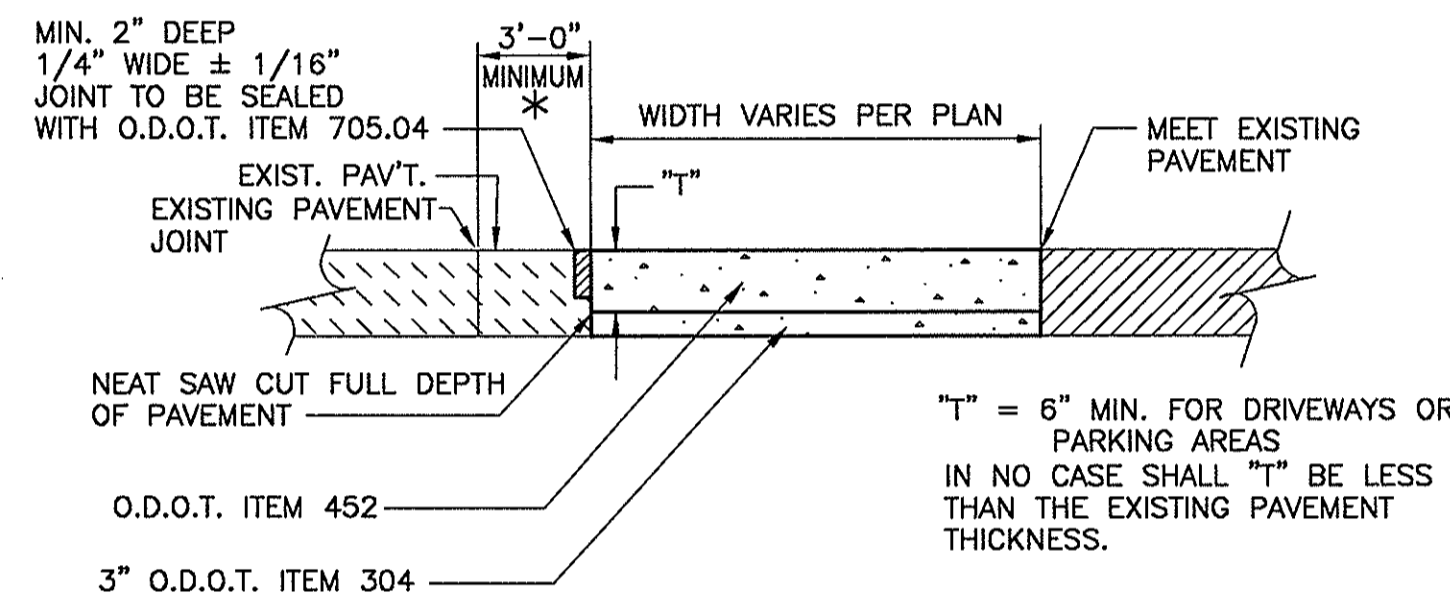
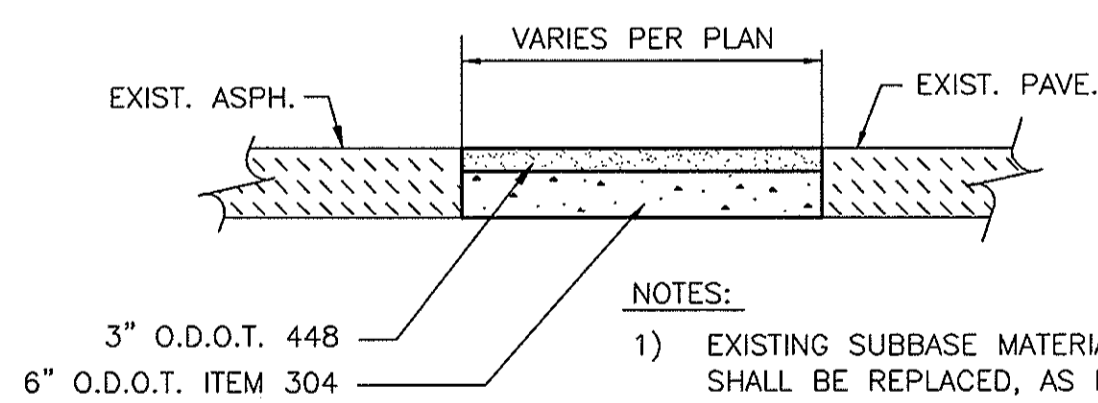


**DRIVEWAY APPROACH DETAIL**  
N.T.S.

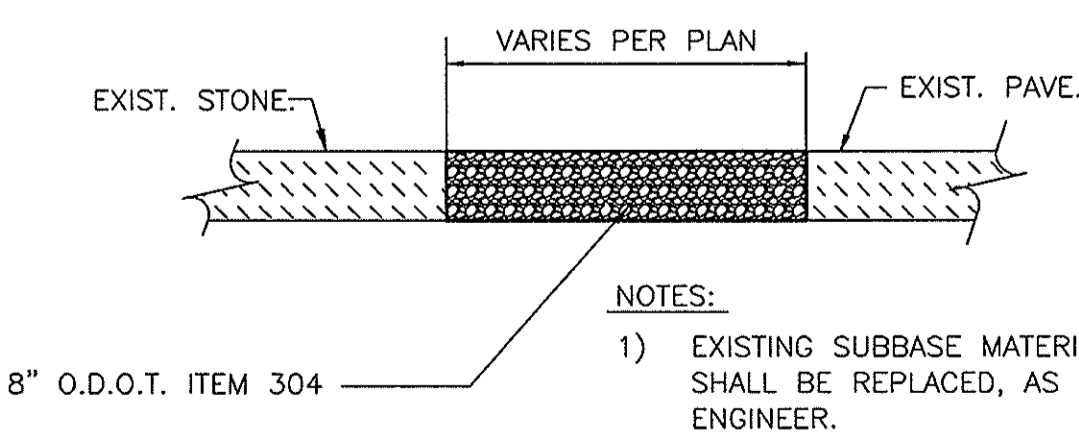


- NOTES:**
- EXISTING SUBBASE MATERIAL (IF ANY) SHALL BE REPLACED, AS DIRECTED BY ENGINEER.
  - REPLACEMENT SHALL BE REINFORCED AS PER O.D.O.T. ITEM 709.10 OR 709.12 IF EXISTING PAVEMENT IS REINFORCED.
  - 5/8" HOOKBOLT @ 30" O.C. MAY BE REQUIRED AS DIRECTED BY THE ENGINEER.
- \* WHERE WIDTH IS LESS THAN 3'-0" OR EXISTING PAVEMENT IS DETERIORATED, THE CONTRACTOR SHALL REPLACE ADDITIONAL PAVEMENT AS DIRECTED BY THE ENGINEER. PAYMENT FOR ADDITIONAL PAVEMENT REPLACEMENT AS DIRECTED BY THE ENGINEER SHALL BE AT THE SAME UNIT PRICE BID.

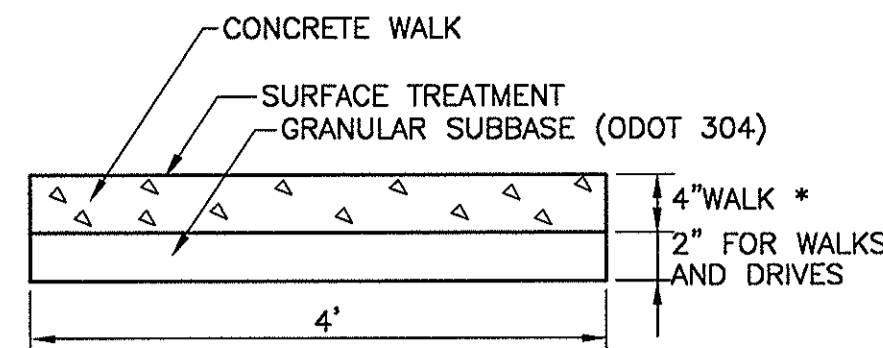
**CONCRETE DRIVE APRON**



**ASPHALT DRIVE APRON**

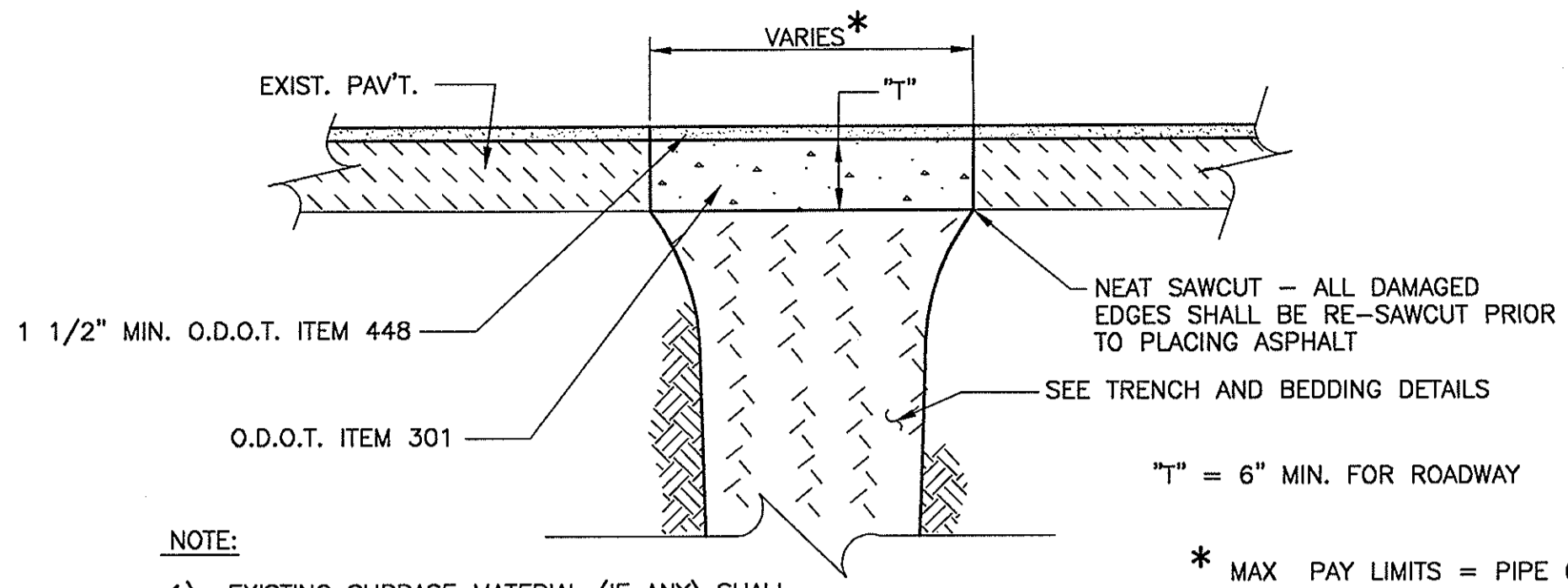


**STONE DRIVE APRON**



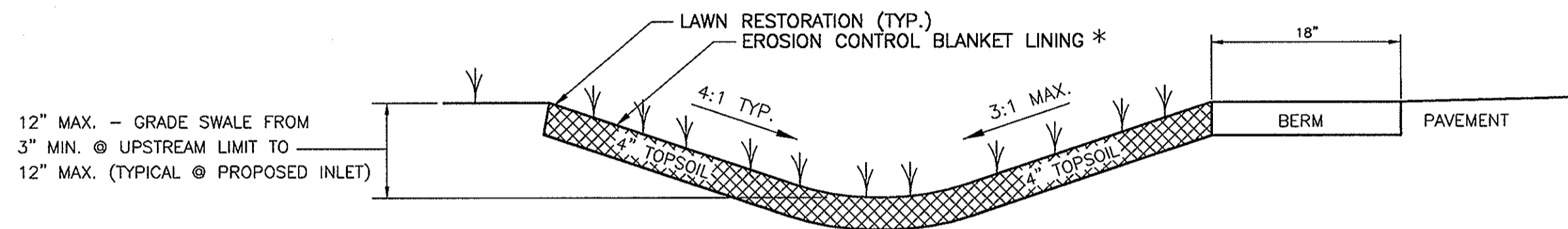
**CONCRETE WALK DETAIL**  
NO SCALE

\*WALK WITHIN DRIVE APRON LIMITS SHALL BE THE SAME THICKNESS AS THE DRIVE APRON (6" MIN.) AND PAID FOR SEPARATELY.  
- 8" FOR COMMERCIAL DRIVES  
- 6" FOR RESIDENTIAL DRIVES



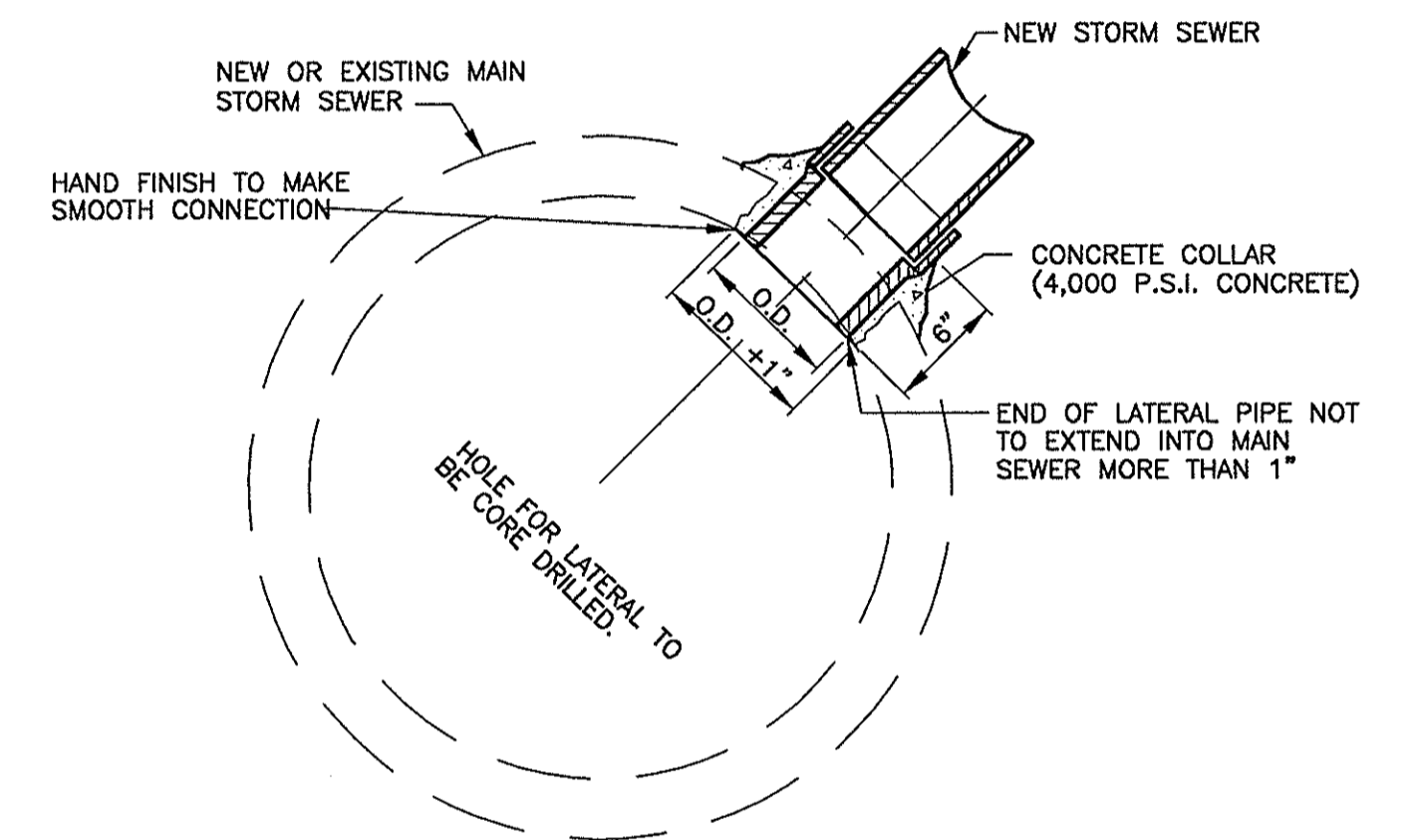
**PAVEMENT/TRENCH REPAIR**  
N.T.S.

- NOTE:**
- EXISTING SUBBASE MATERIAL (IF ANY) SHALL BE REPLACED, AS DIRECTED BY ENGINEER.

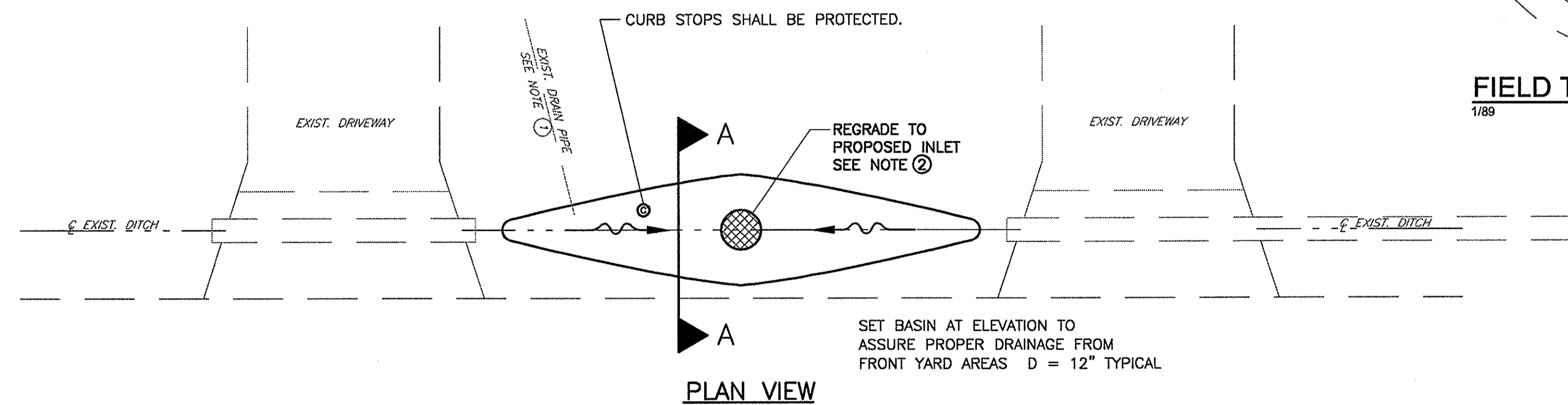


**SECTION A-A**  
**GRASSED SWALE**  
N.T.S.

- \* THE FOLLOWING TYPES OF BLANKETS SHALL BE USED BASED ON THE LONGITUDINAL SLOPE (ROAD PROFILE) AS FOLLOWS:
- SLOPE < 1.5% TYPE B
  - SLOPE ≥ 1.5% TYPE C
  - SLOPE ≥ 5% TYPE E



**FIELD TEE CONNECTION**  
SD-2-15



**PLAN VIEW**  
**SECTION**  
**LINEAR GRADING DETAILS**  
NO SCALE

- ANY STORM WATER PIPE CONNECTION TO THE EXISTING DITCH OR PIPES SHALL BE PROVIDED A FREE OUTLET TO A PROPOSED SWALE, CATCH BASIN OR STORM SEWER AS DIRECTED. A CONTINGENCY QUANTITY OF 6" CONDUIT TYPE C HAS BEEN PROVIDED FOR THIS PURPOSE.
- THE CITY SHALL RESERVE THE RIGHT TO MODIFY DITCH INLET LOCATIONS AND ELEVATIONS AFTER INITIAL CONSTRUCTION LAYOUT STAKING IS COMPLETED.

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REV. NO.	DESCRIPTION	DATE	BY	CHK'D.

DATE: JULY 2010
DRAWN BY: LML
CHECKED BY: TRL
APPROVED BY: TRL
F.B. No. 1221 Pg. 110-156

SCALE	
HOR. AS NOTED	VERT.
CONTRACT No.	
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SHEET NO.	OF
9	11