

LEGEND

- ① EXISTING PAVEMENT
- ② ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE (1 1/4" ±)
- ③ ITEM 407 - TACK COAT (SEE NOTE NO. 2)
- ④ ITEM SPL - ASPHALT CONCRETE, MISC. - 1/2" STRESS ABSORBING MEMBRANE INTERLAYER (SAMI), TYPE 1
- ⑤ ITEM 441 - ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG 64-22 (1 1/2")(448)
- ⑥ ITEM 441 - ASPHALT CONCRETE MISC.: ASPHALT REJUVENATING AGENT

NOTES:

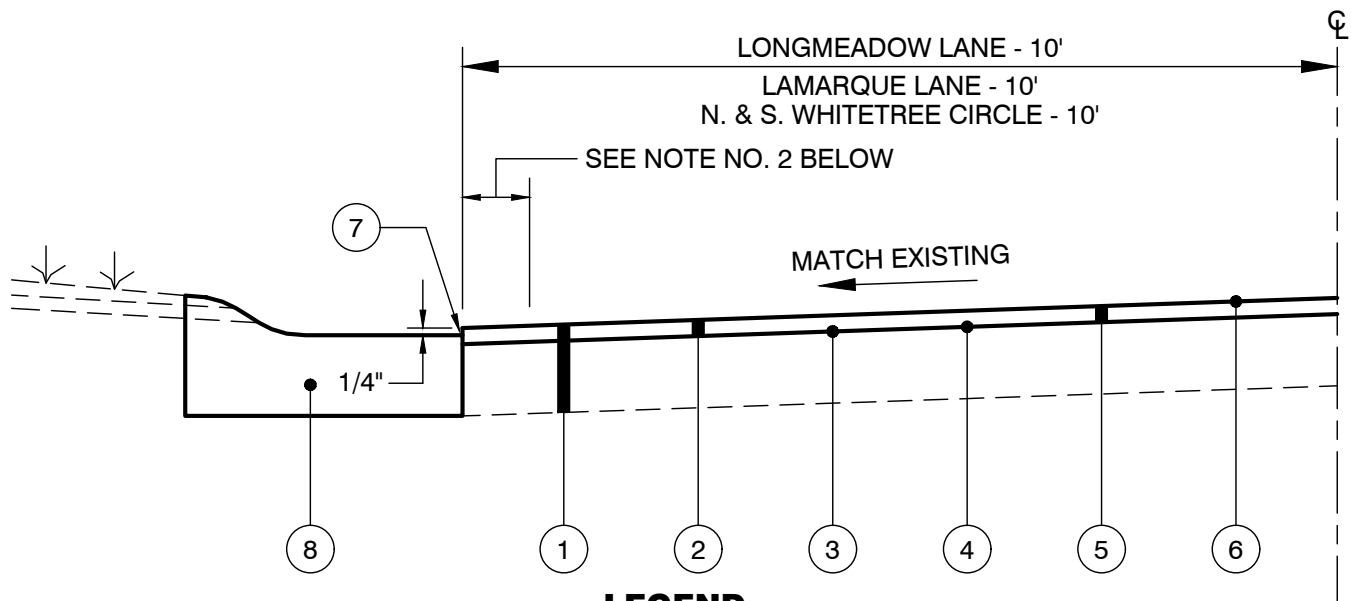
- 1.) EXISTING PAVEMENT SHALL BE COLD PLANED TO 1-1/4" BELOW THE PROPOSED GUTTER PLATE. PAYMENT SHALL BE ONLY FOR THE AREA PLANED.
- 2.) ASPHALT CONCRETE PRICE ADJUSTMENTS AS NOTED IN ODOT CONSTRUCTION AND MATERIAL SPECIFICATION SECTION 401.20 WILL NOT APPLY TO THIS CONTRACT. NO ADJUSTMENTS TO UNIT PRICES WILL BE MADE DUE TO FLUCTUATIONS OF THE COST OF PETROLEUM-BASED PRODUCTS.

TYPICAL PAVEMENT SECTION WITHOUT CURB



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- ⑥ ITEM 441 - ASPHALT CONCRETE MISC.: ASPHALT REJUVENATING AGENT
- ⑦ 3" WIDE SEAL OF ASPHALT BINDER WHERE ASPHALT MEETS CURB FOLLOWING THE PLACEMENT OF ITEM 448 SURFACE COURSE.
- ⑧ ITEM 609 - COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT, MARKED IN FIELD BY OWNER

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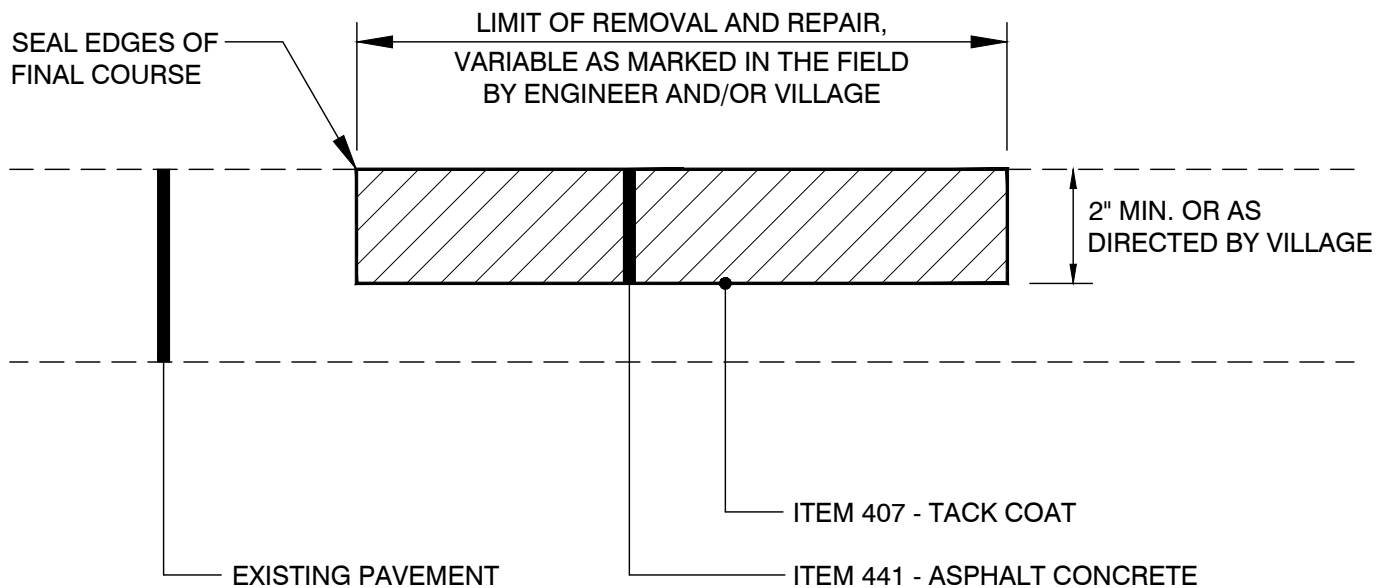
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TYPICAL PAVEMENT SECTION WITH CURB



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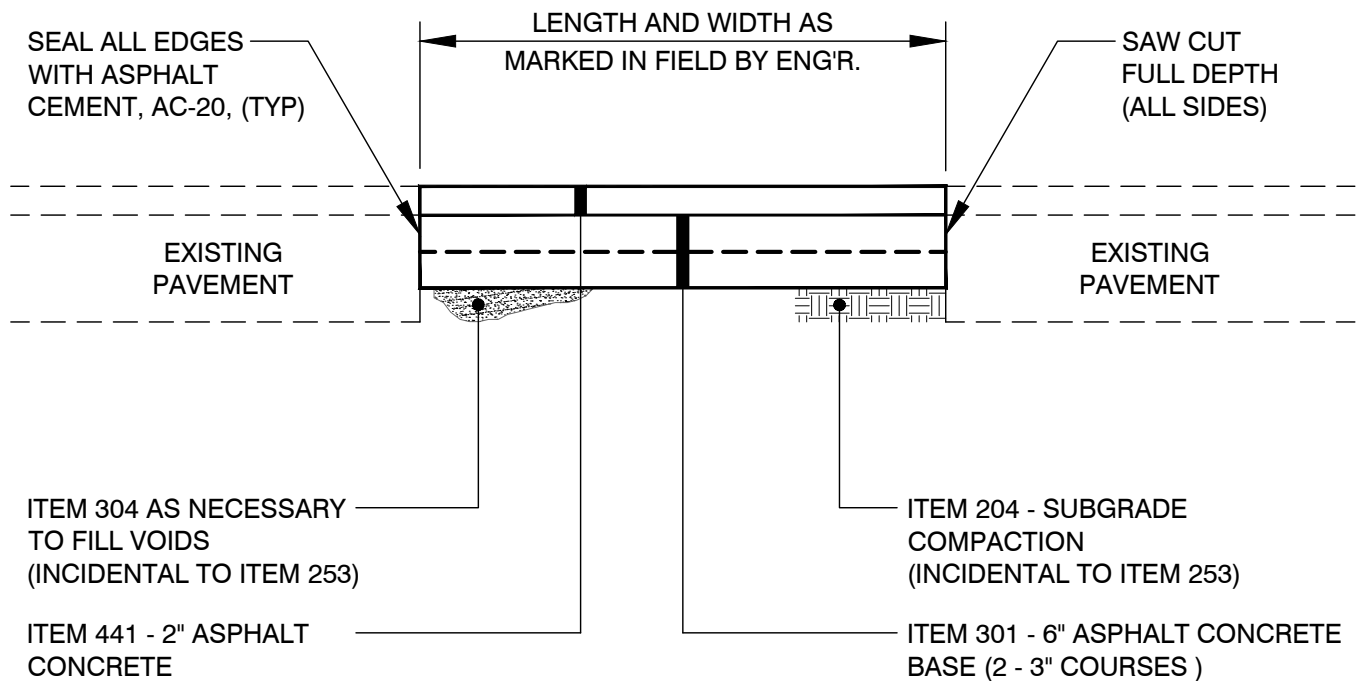
- 1.) REMOVE DETERIORATED PAVEMENT TO THE DEPTH SPECIFIED BY THE ENGINEER SQUARING THE EDGES OF THE AREA TO BE REPAIRED. COAT THE SURFACE AREA WITH 407 MATERIAL BEFORE PLACING ITEM 448.
- 2.) THE CONTRACTOR SHALL REPAIR ONLY THOSE AREAS MARKED IN THE FIELD BY THE ENGINEER AND/OR VILLAGE.

ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR



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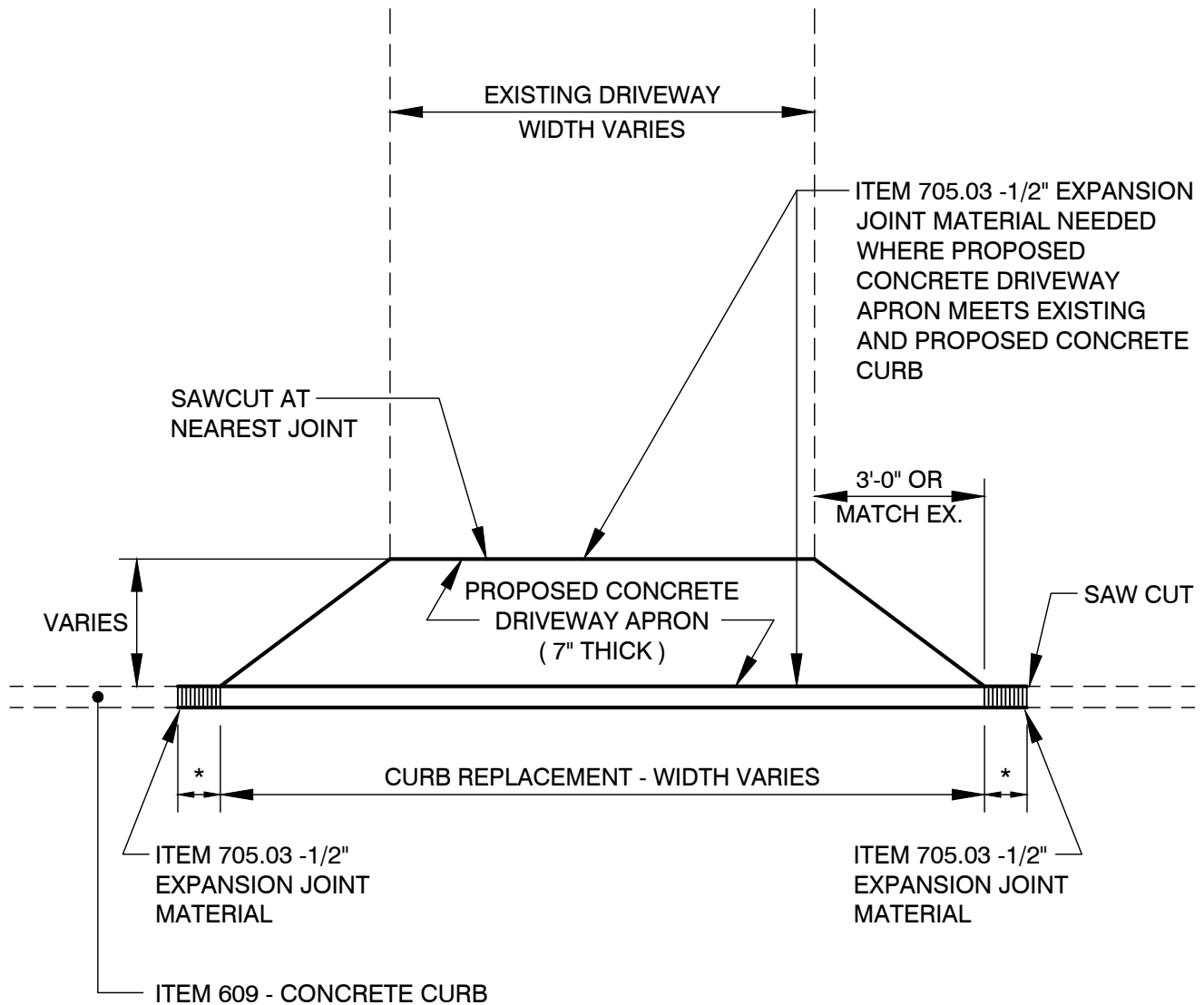
- 1.) AN ADDITIONAL QUANTITY OF ITEM 253 - PAVEMENT REPAIR IS INCLUDED IN THE CONTRACT TO REPLACE ANY ADDITIONAL BASE REPAIR AREAS REQUIRED PRIOR TO PAVING.
- 2.) THE CONTRACTOR SHALL REPAIR ONLY THOSE AREAS MARKED IN THE FIELD BY THE ENGINEER.

ITEM 253 - PAVEMENT REPAIR DETAIL



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ITEM 452 - NON-REINFORCED CONCRETE PAVEMENT - DRIVE APRON

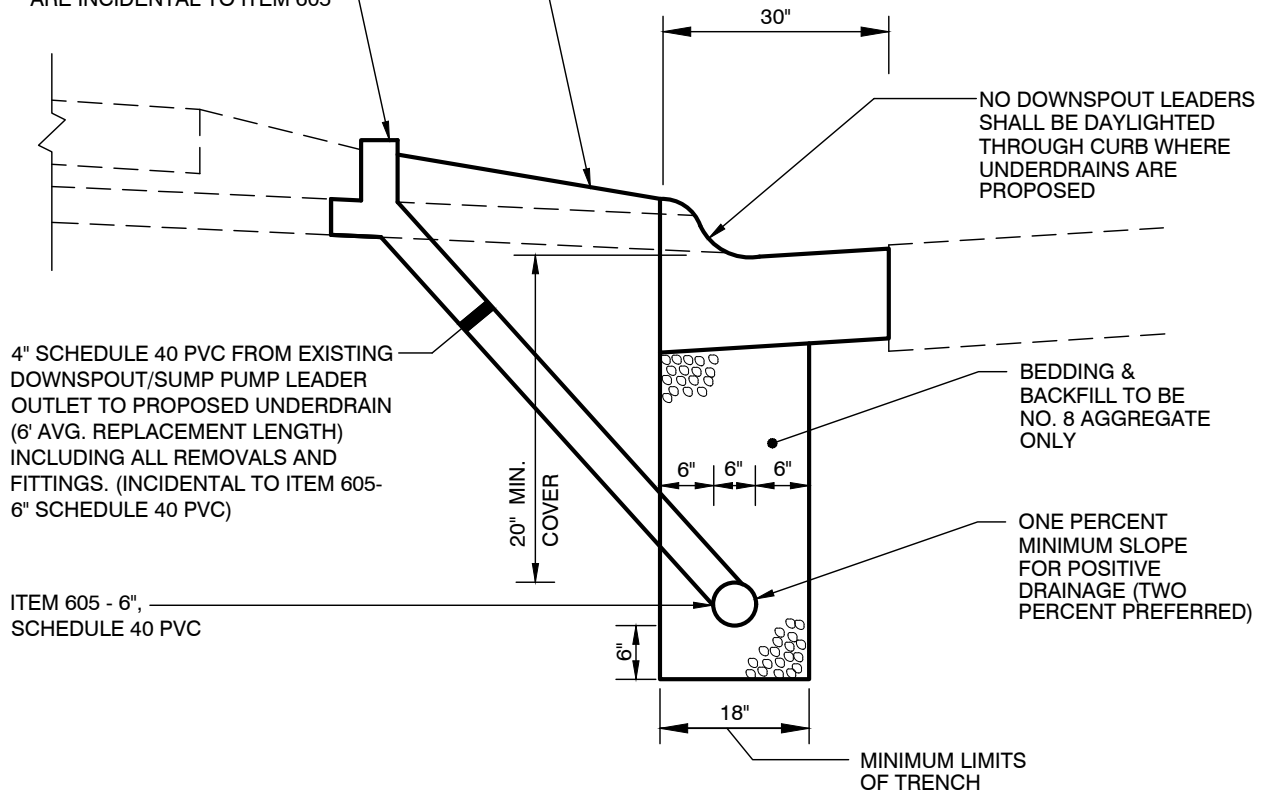


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GRASS AND TOPSOIL RESTORATION INCIDENTAL TO THE APPROPRIATE BID ITEM. ALL RESTORATION WORK IS TO BE COMPLETED WITHIN ONE (1) WEEK OF THE COMPLETION OF THE CONSTRUCTION WORK.

4" CLEANOUT WITH FLUSH STYLE CLEANOUT PLUG SET LEVEL WITH GRADE WYE'S, CLEANOUTS AND 45° BENDS ARE INCIDENTAL TO ITEM 605



NOTES:

- 1.) CONTRACTOR TO ADJUST ALIGNMENT OF THE UNDERDRAIN WHERE NECESSARY (UNDER THE SUPERVISION OF THE CITY INSPECTOR) BOTH HORIZONTALLY AND VERTICALLY TO AVOID GAS, WATER SERVICES AND DRIVE APRONS THAT ARE NOT SCHEDULED FOR REPLACEMENT.
- 2.) ALL PIPE AND FITTINGS TO BE SCHEDULE 40 PVC GLUED AT CONNECTIONS.
- 3.) AT CURVES IN ROADWAY MAXIMUM OF 45° BENDS SHALL BE UTILIZED.

ITEM 605 - COLLECTOR LINE & DOWNSPOUT CONNECTION (UNDER CURB)



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GRASS AND TOPSOIL RESTORATION INCIDENTAL TO THE APPROPRIATE BID ITEM. ALL RESTORATION WORK IS TO BE COMPLETED WITHIN ONE (1) WEEK OF THE COMPLETION OF THE CONSTRUCTION WORK.

4" CLEANOUT WITH FLUSH STYLE CLEANOUT PLUG SET LEVEL WITH GRADE WYE'S, CLEANOUTS AND 45° BENDS ARE INCIDENTAL TO ITEM 605

NO DOWNSPOUT LEADERS SHALL BE DAYLIGHTED THROUGH CURB WHERE UNDERDRAINS ARE PROPOSED

4" SCHEDULE 40 PVC FROM EXISTING DOWNSPOUT/SUMP PUMP LEADER OUTLET TO PROPOSED UNDERDRAIN (6' AVG. REPLACEMENT LENGTH) INCLUDING ALL REMOVALS AND FITTINGS. (INCIDENTAL TO ITEM 605-6" SCHEDULE 40 PVC)

BEDDING & BACKFILL TO BE NO. 8 AGGREGATE ONLY

ONE PERCENT MINIMUM SLOPE FOR POSITIVE DRAINAGE (TWO PERCENT PREFERRED)

ITEM 605 - 6", SCHEDULE 40 PVC

20" MIN. COVER

6"

6" 6" 6"

18"

MINIMUM LIMITS OF TRENCH

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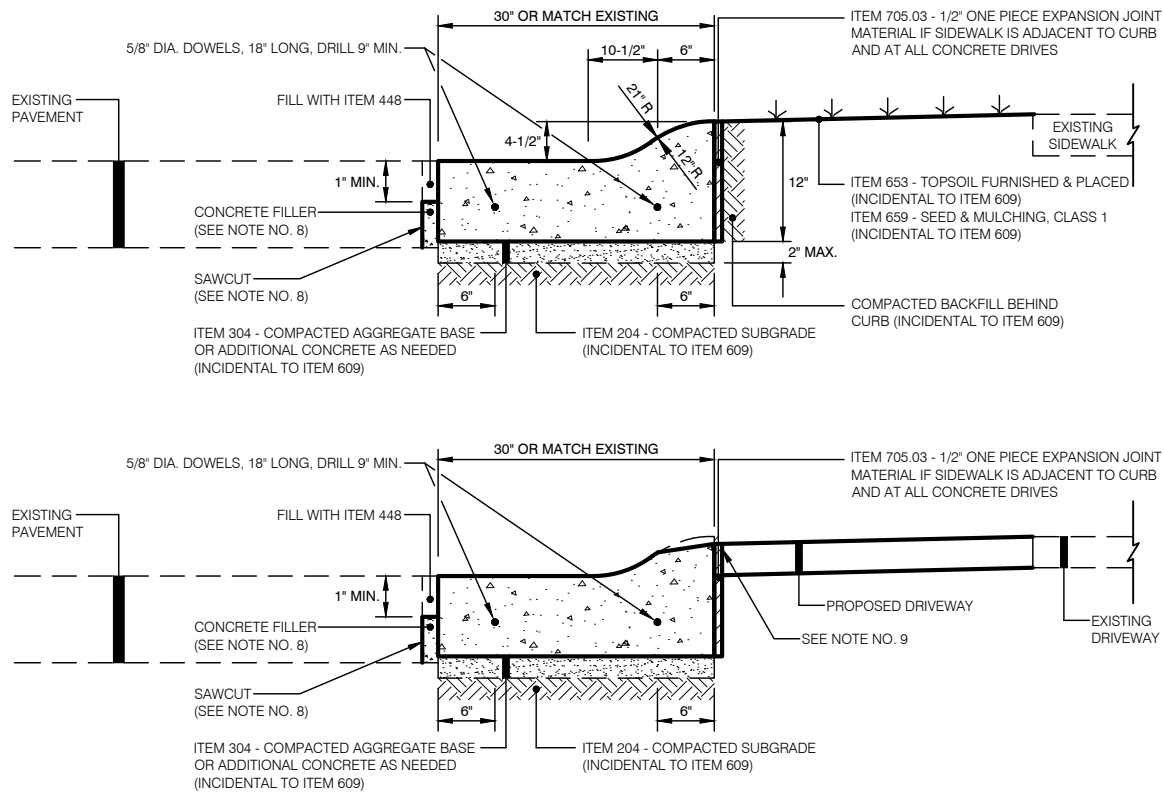
- 1.) CONTRACTOR TO ADJUST ALIGNMENT OF THE UNDERDRAIN WHERE NECESSARY (UNDER THE SUPERVISION OF THE CITY INSPECTOR) BOTH HORIZONTALLY AND VERTICALLY TO AVOID GAS, WATER SERVICES AND DRIVE APRONS THAT ARE NOT SCHEDULED FOR REPLACEMENT.
- 2.) ALL PIPE AND FITTINGS TO BE SCHEDULE 40 PVC GLUED AT CONNECTIONS.
- 3.) AT CURVES IN ROADWAY MAXIMUM OF 45° BENDS SHALL BE UTILIZED.

ITEM 605 - COLLECTOR LINE & DOWNSPOUT CONNECTION (BEHIND CURB)



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GENERAL NOTES:

- 1.) CONCRETE SHALL BE 4000 PSI @ 28 DAYS WITH 5% MIN. TO 8% MAX. ENTRAINED AIR.
- 2.) MAINTAIN SPACING OR EXISTING EXPANSION AND CONTRACTION JOINTS.
- 3.) THE COST OF ANY REQUIRED YARD RESTORATION WHICH WOULD INCLUDE TOPSOIL AND/OR SEEDING AND MULCHING SHALL BE INCIDENTAL TO ITEM 609, CONCRETE CURB AND GUTTER, REMOVE AND REPLACE.
- 4.) THE MIN. CURB AND GUTTER REPLACEMENT SHALL BE 3'-0" WITH EXPANSION MATERIAL AND DOWELS PLACED ON AT LEAST ONE END OF THE REPAIRED SECTION.

HAND / MACHINE FORMED CURB

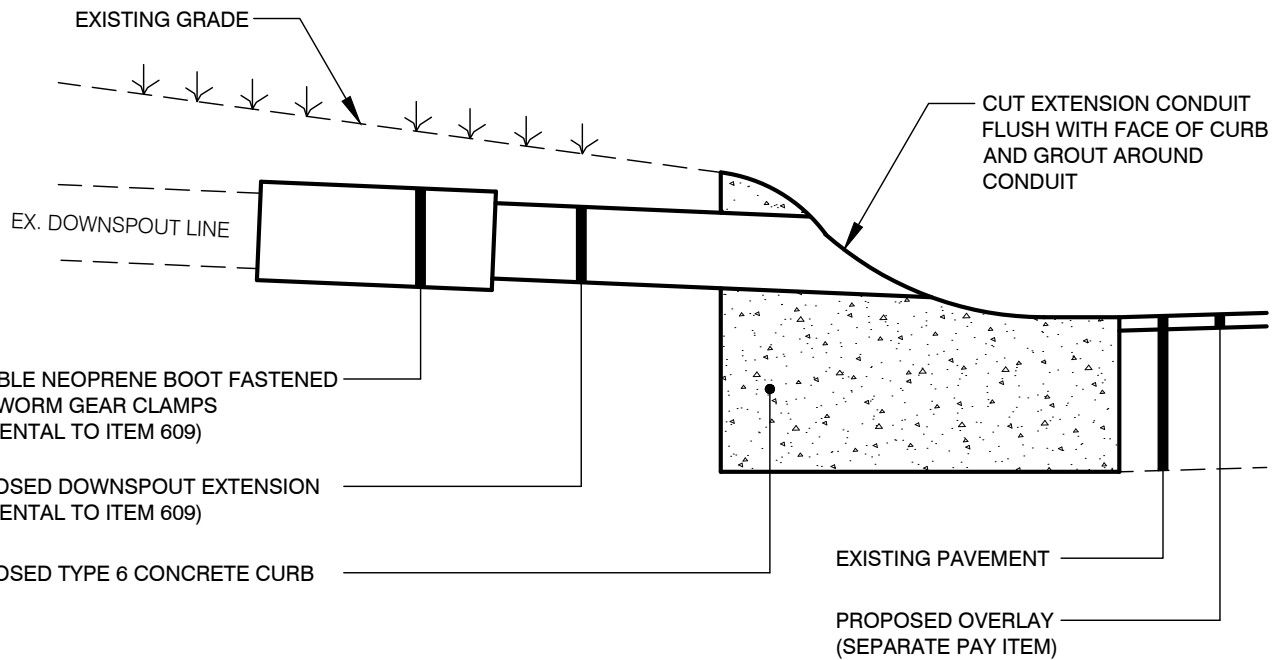
- 5.) DOWELING WILL BE REQUIRED AT EXPANSION JOINTS IN THE NEW CURB. TWO DOWELS SHALL BE PLACED PER DETAIL. EXPANSION JOINTS SHALL BE PLACED:
 - A.) AT A MAXIMUM SPACING OF 40' FOR HAND FORMED CURB.
 - B.) ONE FOOT ON EACH SIDE OF CATCH BASIN PER ODOT STANDARD DRAWINGS CB-3 AND CB-3A.
 - C.) ALL CURB RADII RETURNS AND AT ALL STOPS IN POURS.
 - D.) WHERE THE NEW CURB MEETS THE EXISTING CURB.
- 7.) THE DIMENSIONS ABOVE MAY BE MODIFIED BY THE ENGINEER IN THE FIELD TO MATCH THE DIMENSIONS OF THE VARIOUS EXISTING CURBS TO BE REMOVED.
- 8.) TO BE USED WHERE NEW CURB DOES NOT MEET EXISTING PAVEMENT.
- 9.) DEPRESS CURB AS REQUIRED TO MATCH AND SAVE THE EXISTING APRON.
- 10.) IF THE CONTRACTOR BREAKS THE APRON WHILE INSTALLING THE CURB, HE SHALL REPLACE THE APRON TO 1' BEHIND THE CURB (FULL WIDTH OF APRON). THE COST SHALL BE INCIDENTAL TO ITEM 609.
- 11.) IF THE APRON WAS SPALLED BEFORE CONSTRUCTION, THE CONTRACTOR SHALL CLEAN AND PATCH THE APRON UP TO 1' BEHIND THE CURB IN ORDER TO PROVIDE A SMOOTH TRANSITION TO THE PROPOSED CURB. THE COST SHALL BE INCIDENTAL TO ITEM 609.
- 12.) WORK PERFORMED ON APRONS WHICH FALL APART AFTER THE CURB IS REMOVED DUE TO NO FAULT OF THE CONTRACTOR (PRE-EXISTING CONDITION) SHALL BE A SEPARATE PAY ITEM.
- 13.) A WHITE PIGMENT CURING COMPOUND SHALL BE APPLIED TO THE NEW CURB AND GUTTER.

ITEM 609 - COMBINATION CURB & GUTTER REPLACEMENT DETAIL



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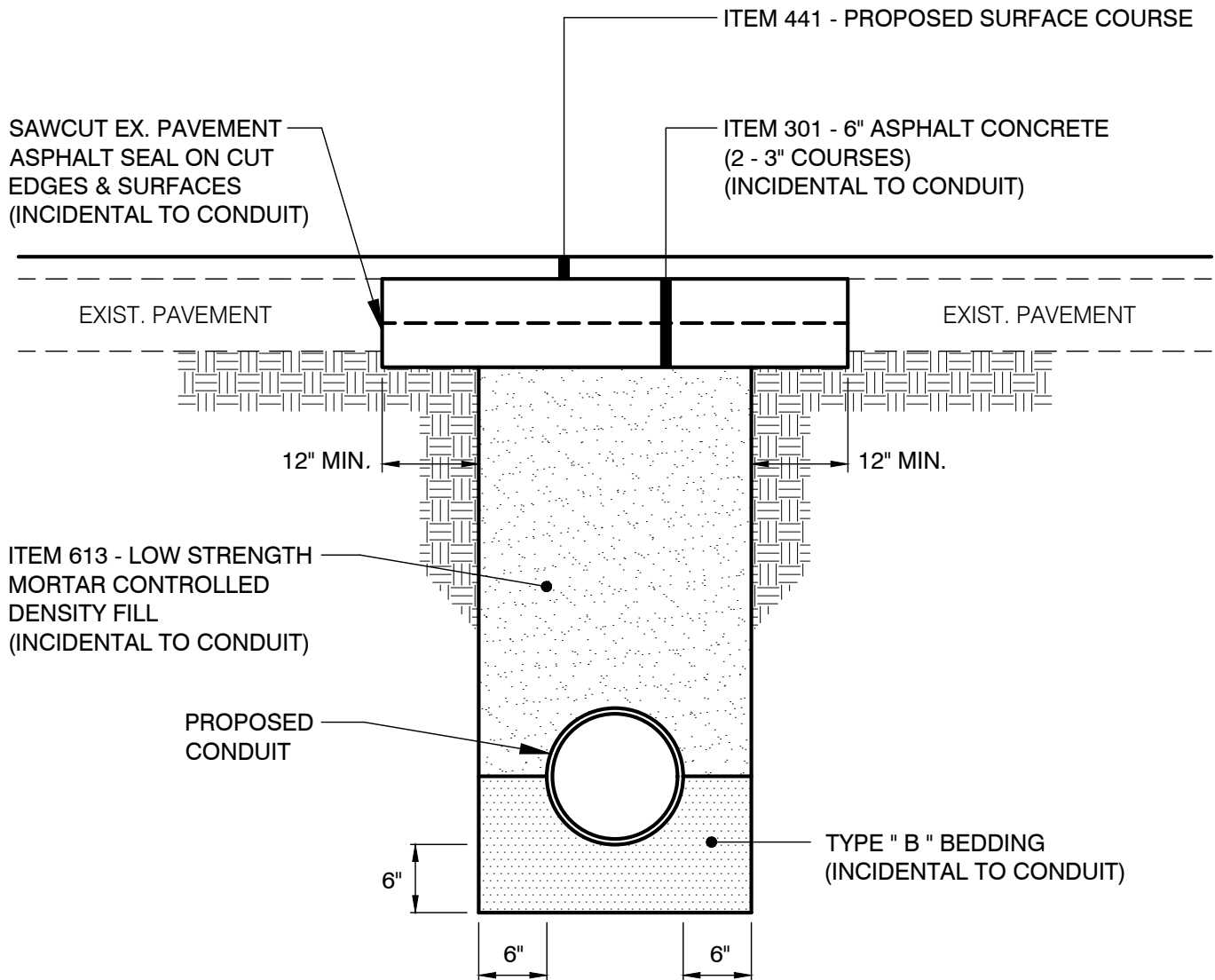
- 1.) 3" PVC PIPE SCH. 40, (SLOPED TO DRAIN) MAY BE CAST-IN-PLACE AT TIME OF CONSTRUCTION WITH 3" SLOT IN CURB. FOR INSTALLATION AFTER CURB IS CONSTRUCTED, THE CURB SLOT MUST BE SAW CUT (4.5" MAX. WIDTH) AND THE AREA FILLED WITH AN EPOXY CONCRETE. TYPE OF EPOXY CONCRETE TO BE APPROVED BY THE OWNER / ENGINEER BEFORE USED BY THE CONTRACTOR.
- 2.) THE COST FOR THE LABOR AND MATERIAL NECESSARY TO REPLACE THE DOWNSPOUT LEADER SHALL BE INCIDENTAL TO THE PERTINENT CURB REPLACEMENT ITEM FOR EACH DOWNSPOUT REPLACED THREE (3) FEET IN LENGTH. TOPSOIL AND SEEDING SHALL BE INCIDENTAL IN COSTS.
- 3.) IF AT THE DIRECTION OF THE ENGINEER, MORE THAN 3' OF DOWNSPOUT DRAIN NEEDS TO BE REPLACED, THE ADDITIONAL WORK SHALL BE PAID ON A PER FOOT BASIS UNDER ITEM 603 - 3" PVC PIPE SCH. 40. ANY NECESSARY VERTICAL OR HORIZONTAL BENDS SHALL BE INCLUDED AND PAID FOR ON THE ABOVE BASIS.

DOWNSPOUT EXTENSION DETAIL



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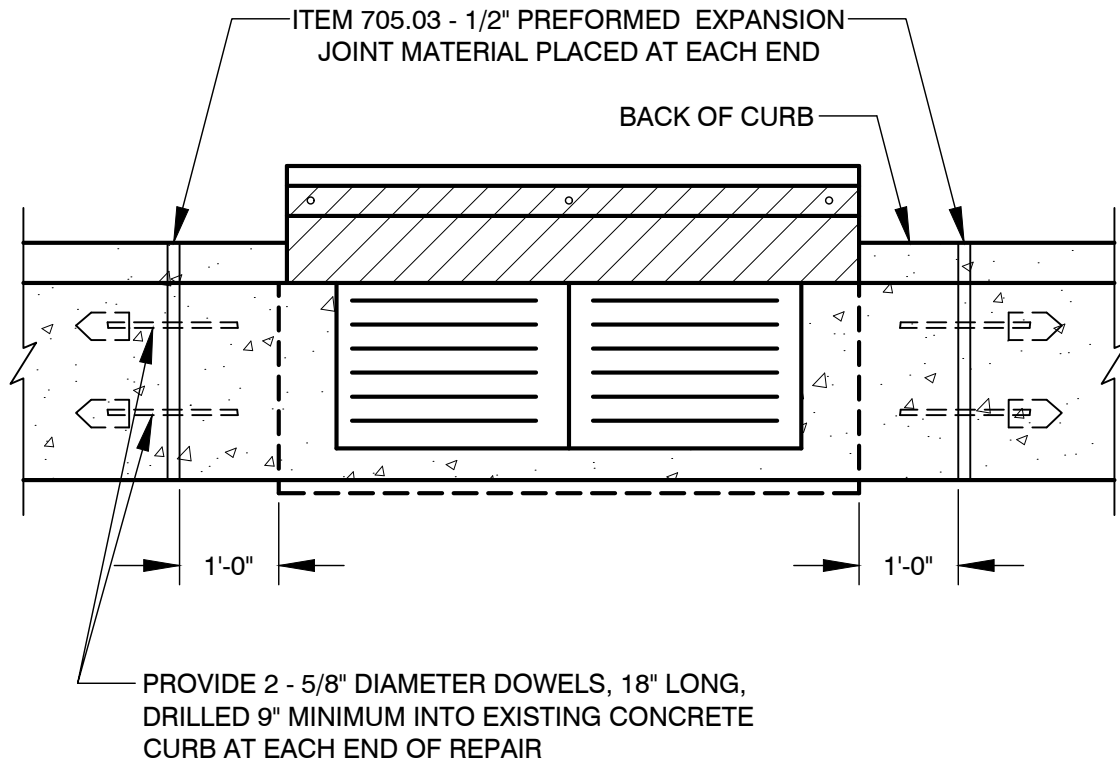


TRENCH RESTORATION



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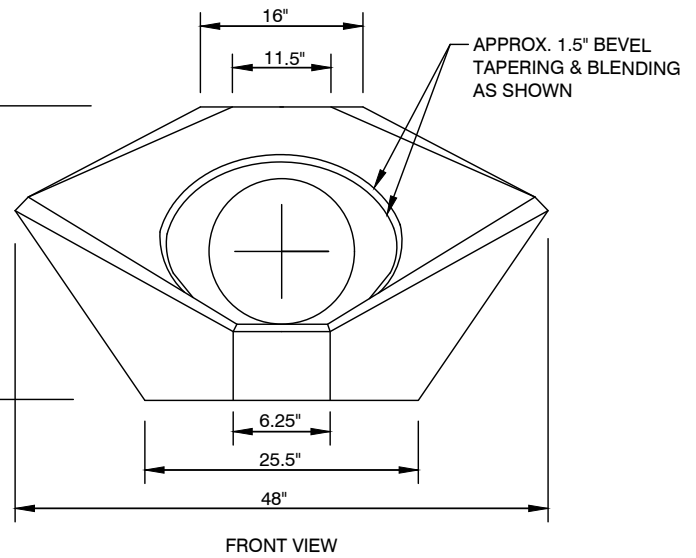
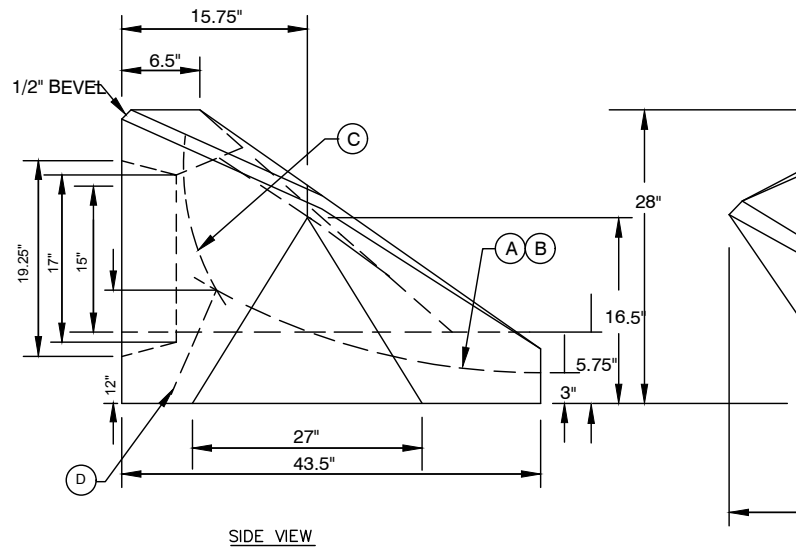
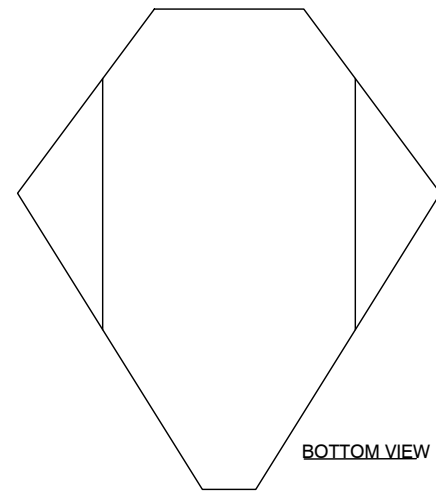
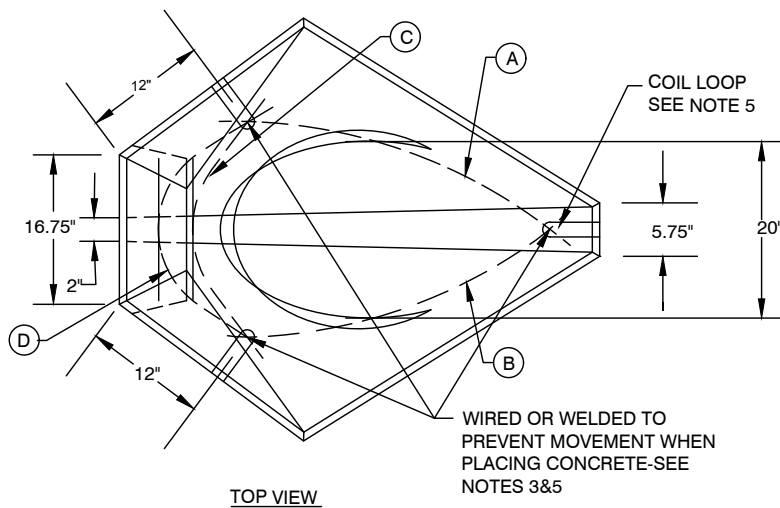
- 1.) SEE ODOT STANDARD DRAWINGS CB-3 & CB-3A

ROLL CURB REPLACEMENT ADJACENT TO CATCH BASIN



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- ① BOTTOM APRON SHALL BE SLOPED IN DIRECTION OF FLOW EQUAL TO SLOPE OF PIPE.
- ② DIMENSIONS AND QUANTITIES ARE BASED ON STEEL PIPE AND WILL VARY SLIGHTLY FOR CONCRETE PIPE.
- ③ DIMENSIONS FROM FACE OF CONCRETE TO STEEL SHALL BE 2" CLEAR DISTANCE.
- ④ HEADWALL LOCATED AT EDGE OF SHOULDER SHALL BE PARALLEL TO CENTERLINE OF ROAD.
- ⑥ ENCIRCLED LETTERS, ○, INDICATE STEEL BAR LOCATIONS. EITHER WIRED OR WELDED AT 3-1"X6" COIL LOOPS AS SHOWN IN TOP & SIDE VIEWS. SEE NOTE 3.
- ⑦ USE CLASS QC1 CONCRETE (OH) - APPROX. .35 CU. YD.
- ⑧ REINF. STEEL APPROX. 13' OR 4.9 LBS. MINIMUM GRADE 40.
- ⑨ APPROX. COMPLETE WEIGHT 1365 LBS.

LOW PROFILE HEADWALL



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